

DUNCAN

KEY DESTINATIONS

- SCALE Trail/Park
- James F. Byrnes High School
- Duncan Elementary School and Beech Springs Intermediate School
- Stoneledge Park
- Shipwreck Cove Water Park
- Duncan Station (residential development)

KEY ISSUES

- Many residents live below poverty line and walking/biking is a primary mode of transportation.
- Crosswalks in front of Byrnes High School are of concern for pedestrian safety.
- Lack of connectivity throughout Duncan and to adjacent towns.
- Intersection of Main Street, Spencer St., and SC 292 impossible for pedestrians.

PRIORITY PROJECTS

1. Main Street bike lane/sharrow.
2. Enhance pedestrian accommodations along Danzler/SC 290 area. A number of destinations are in this area including the school, SCALE park, new performing arts venue, and other connections across SC 290. Sidewalks should be extended. Signage and marked crosswalks should be improved with curb ramps, speed limit reduction, and traffic calming.
3. Spencer Street paved shoulder.

PRIORITY INTERSECTION

Danzler and SC 290

- Stripe new high-visibility crosswalks.
- Provide advanced stop lines.
- Reconstruct existing curb ramps.
- Provide median refuge islands.
- Reduce turning radius with curb extensions (potential impact to trucks).
- Add pedestrian countdown signals.
- Provide high-visibility pedestrian warning signs.
- Provide in-roadway pedestrian warning signs.



Above: SCALE Trail sign.



Above: Stoneledge Park

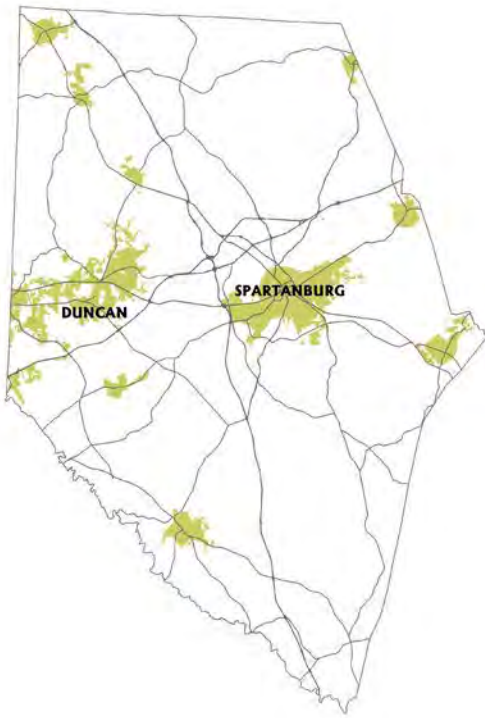


Above: Main Street bicycle lane opportunity



Above: Intersection of Danzler and SC 290

INSET MAP: DUNCAN



Above: existing conditions between the SCALE Trail/Park and the school.

Below: A photo visualization of pedestrian improvements. (Note: As of late 2009, this crosswalk is being considered for the other entrance to SCALE; image below serves as a conceptual example only).



The intersection of SC 290 and SC 296 is very dangerous for pedestrians. Wide turning radii and long crossing distances make it difficult to navigate for pedestrians. Reducing the curb radius, adding a median refuge island, making the crosswalk more visible, and improving the pork chop island would improve this intersection dramatically.



MAP 6.24 DUNCAN BICYCLE RECOMMENDATIONS



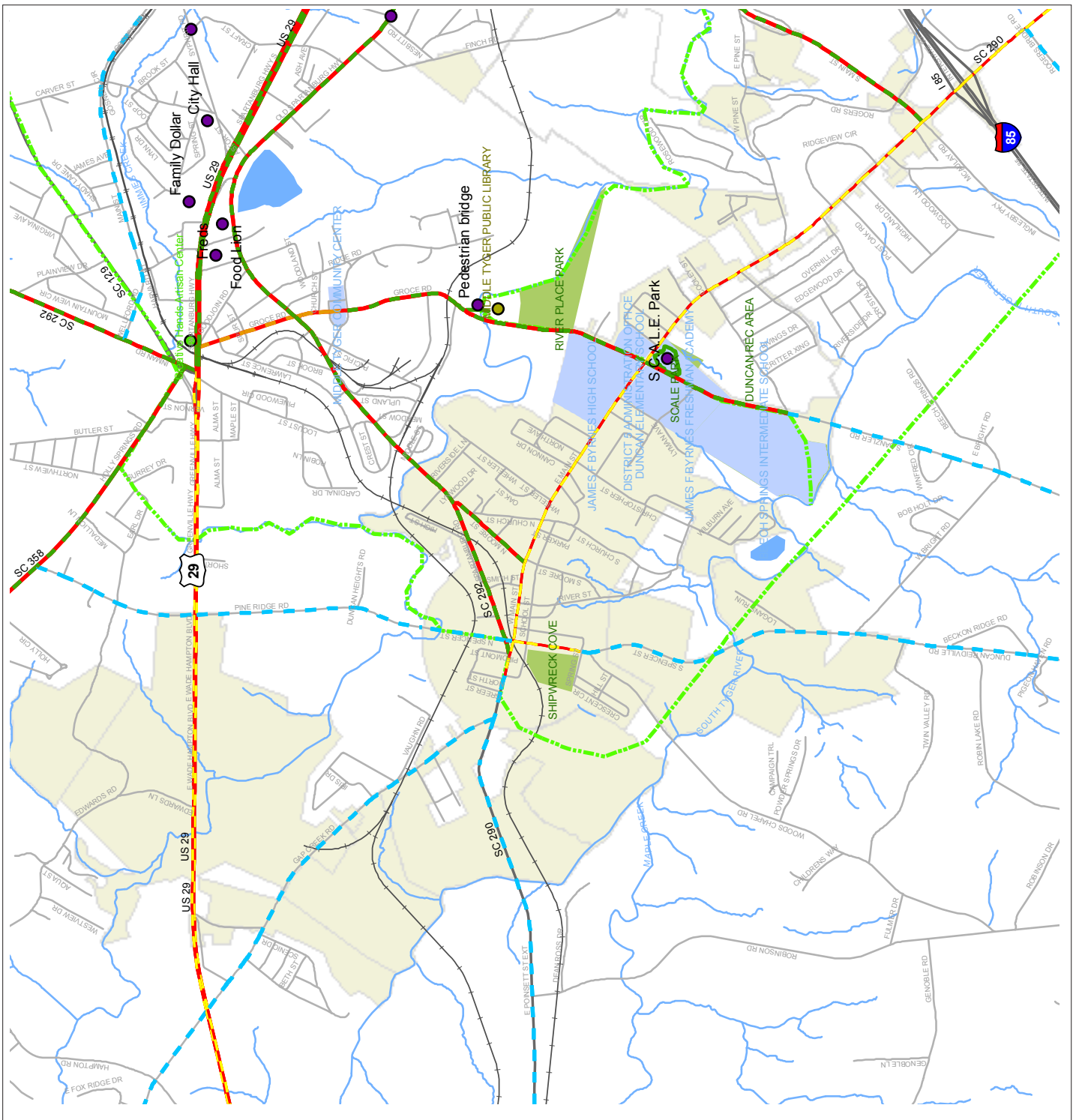
Legend

- Cultural Tourism
- Libraries
- Destinations

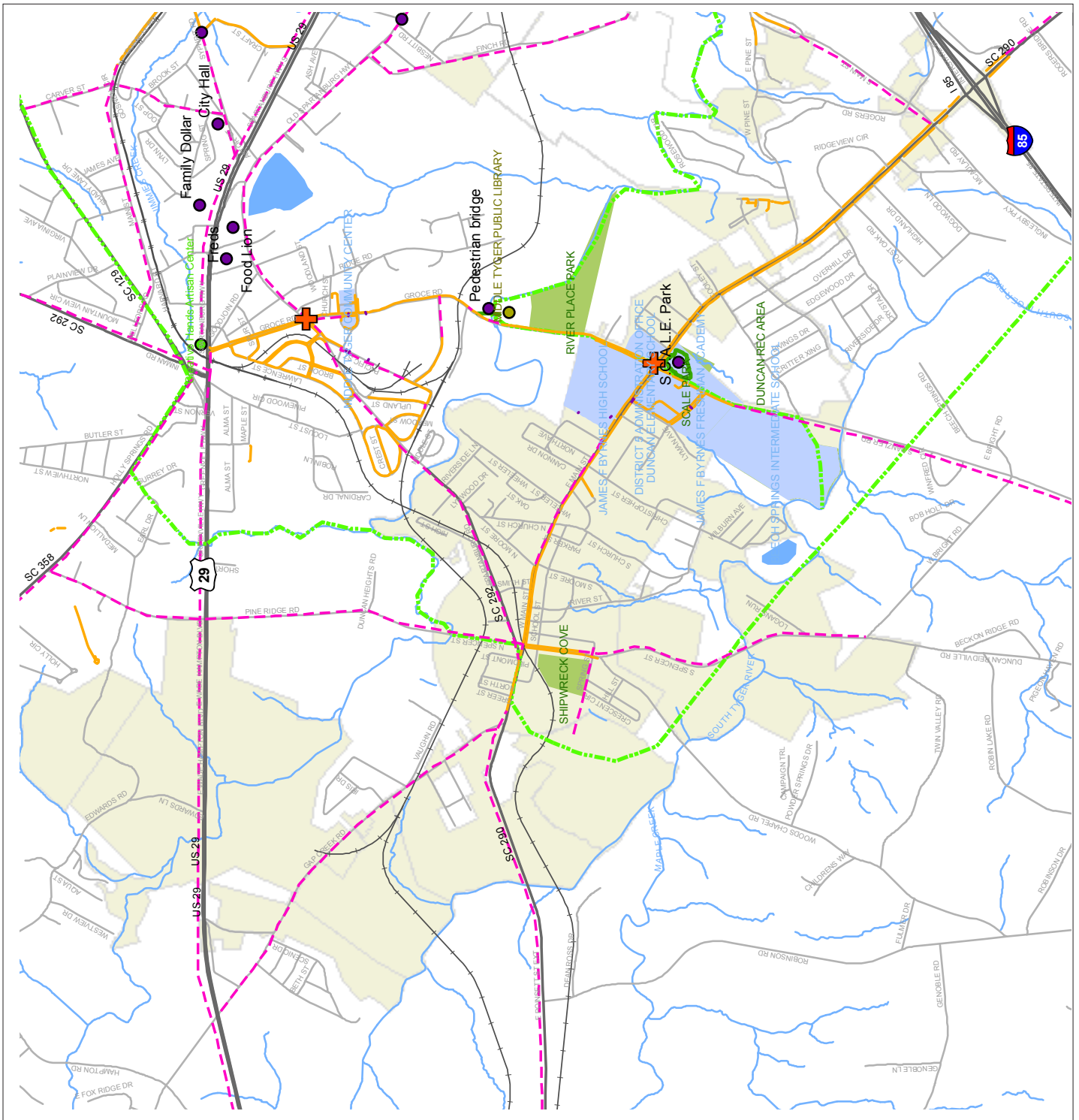
Recommended Bicycle Facilities

- Bike Lane, Restripe
- Bike Lane, New Const
- Paved Shoulder, New Const
- Road
- Existing Trails
- - - Enhancement Plan Rec. Trails
- Rail Road
- Streams and Lakes
- Schools
- Parks
- Water
- County Boundary

Data Source: SPATS



MAP 6.25 DUNCAN PEDESTRIAN RECOMMENDATIONS



Legend

- Intersection Improvements
- Cultural Tourism
- Libraries
- Destinations

Recommended Pedestrian Facilities

- Existing Crosswalks
- Existing Sidewalk
- Recommended Sidewalk
- Existing Trails
- Enhancement Plan Rec. Trails
- Rail Road
- Streams and Lakes
- Schools
- Parks
- Water
- County Boundary

Data Source: SPATS

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